

POWER STEERING-GENERAL

FITTING NEW BELTS

On some engines it will be necessary to remove the alternator drive belt before removal of the steering pump belt is possible.

When a new belt has been fitted it is advisable to check and if necessary adjust the tension after short period of running. This is usually necessary as a new belt needs to 'Bed-In' to its drive pulleys.

The tension should always be checked using light finger pressure in the centre of the longest run.

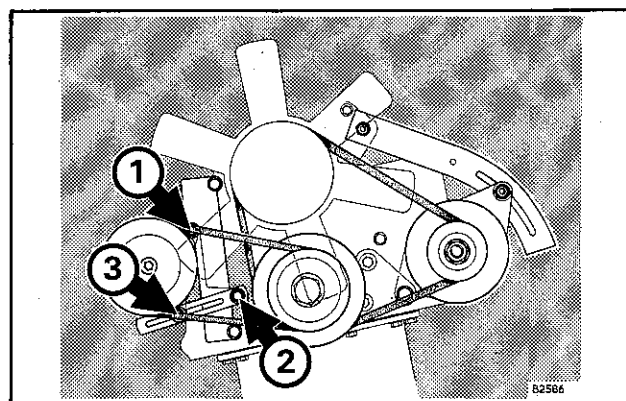


Fig. 1 2 litre engine

RG 225 Engine

Release 'Dzus' fasteners and remove front under-tray.

From below, slacken the pump pivot bolt (1).

From above, slacken the adjustment strap pivot bolt (2) and slide bolt (3).

Move the pump away from the engine until the correct belt tension of 9mm — 12mm (3/8in — 1/2in) is obtained.

Hold the pump in this position and re-tighten the slide bolt.

Re-tighten the pump and strap pivot bolts.

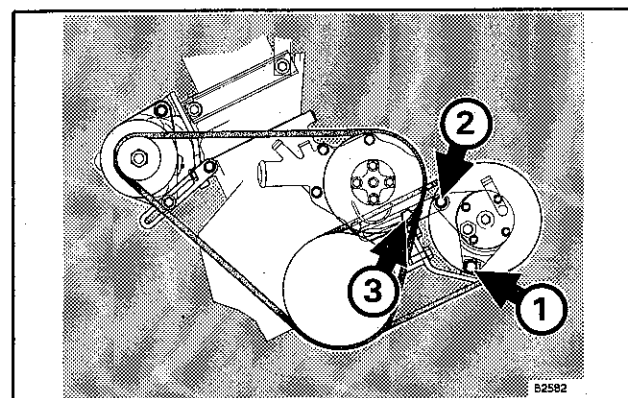


Fig. 2 RG 225 engine

BELT ADJUSTMENT

2 Litre Engine

Release 'Dzus' fasteners and remove front under-tray.

From below, slacken the adjustment strap pivot (2) and slide bolts (3).

From above, slacken the pump pivot bolt (1).

Move the pump away from the engine until the correct belt tension of 9mm — 12mm (3/8in — 1/2in) is obtained.

Hold the pump in this position and re-tighten the slide bolt.

Re-tighten the pump and strap pivot bolts.

4.236 Engine

Release 'Dzus' fasteners and remove front under-tray.

From below, slacken the adjustment strap pivot bolt (2) and slide bolt (3).

From above, slacken the pump pivot bolt (1).

Move the pump away from the engine until the correct belt tension of 9mm – 12mm (3/8in – 1/2in) is obtained.

Hold the pump in this position and re-tighten the slide bolt.

Re-tighten the pump and strap pivot bolts.

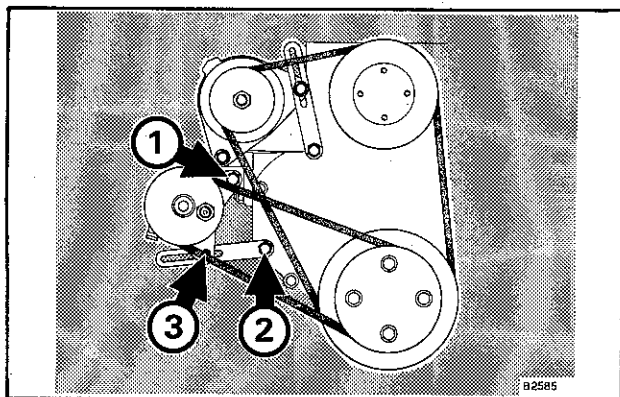


Fig. 3 4.236 engine

Note: As the bolt (2) also secures the exhaustor (B) the exhaustor drive belt tension must be checked and if necessary adjusted.

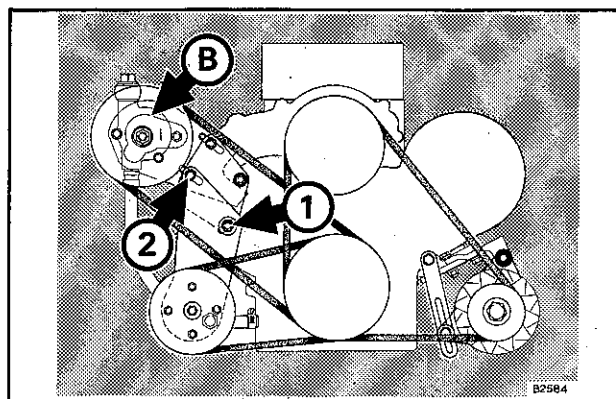


Fig. 4 6.247 engine (Except RB 75)

6.247 Engine (RB75 only)

When the 6.247 engine is fitted to the R875 model the power steering pump is fitted to, and driven by, the compressor. The compressor and pump assembly is driven by twin drive belts from the crankshaft pulley. Adjust as follows:

Release 'Dzus' fasteners and remove front under-tray.

Slacken two set screws securing compressor top stay.

Slacken three nuts securing compressor to front engine mounting.

Turn the adjusting bolt to move the compressor until the belts reach the correct tension of 9mm (3/8in).

Tighten the three nuts and two setscrews securing the compressor.

Refit the engine sound insulation panel and front undertray.

6.247 Engine (Except RB75)

Release 'Dzus' fasteners and remove front under-tray.

Slacken bolts (1) and (2) shown in Fig. 4. Note that bolt (2) also secures the exhaustor (B).

From below, use a lever to move the pump away from the engine until the correct belt tension of 9mm – 12mm (3/8in – 1/2in) is obtained.

Hold the pump in this position and re-tighten the two bolts.