

PIPES AND HOSES

All models except RB75 with 6.247 engine incorporate three separate pipe/hose assemblies. The larger of the two hoses conveys fluid from the reservoir to the pump and is secured at each end by hose clips. The shortest hose and pipe assembly supplies fluid, under pressure, from the pump to the steering box. This pipe is secured at both ends by unions. The fluid is returned from the steering box to the reservoir through another long pipe/hose assembly. It is fitted with a union at the steering box and secured by a hose clip at the reservoir.

To Remove

Position a suitable clean receptacle adjacent to the lowest point of disconnection to catch the fluid which will drain when the pipe or hose is removed. This will avoid unnecessary loss of fluid.

Repeat the above operation for each pipe or hose to be removed.

Unscrew the pipe union or slacken the hose clip of the pipe or hose to be removed and drain the fluid.

If the supply or return hoses are to be removed, it will be necessary to remove the two clips securing the hoses to the front chassis cross-member. Each clip is secured with a nut, bolt and washer.

Note: a) Some fluid will remain in the hoses at their lowest point i.e. At the point where they lay on the front chassis cross-member. To drain this, raise the hose commencing at the reservoir end.

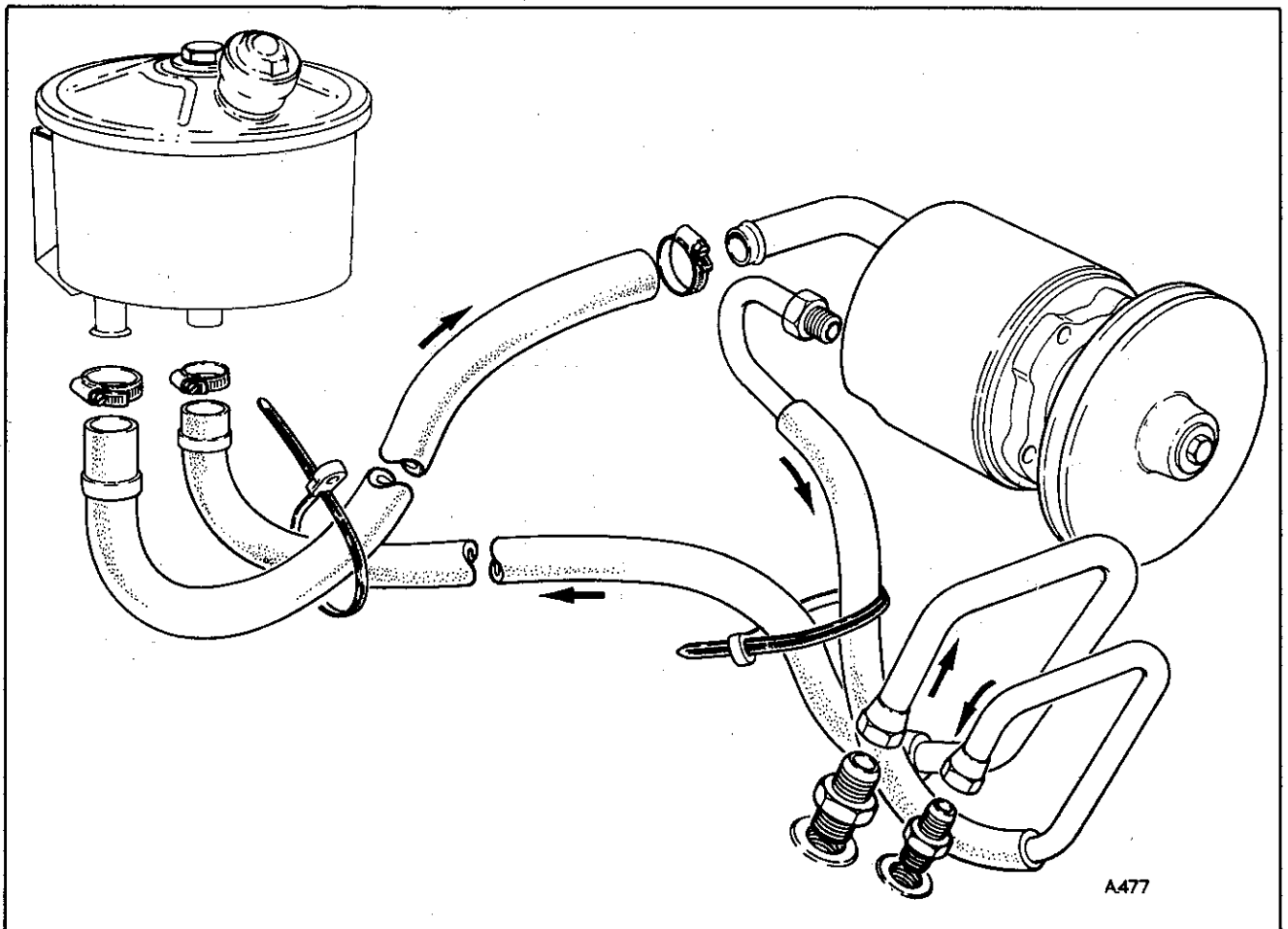


Fig. 1 Layout of Pipework (except RB75 with 6.247 engine)

- b) The fluid used has a detrimental effect on paintwork and due care should therefore be exercised to prevent contact between the two.

To Refit

Refitting is a reversal of the removal instructions.

Bleed the system (Refer to Sub-section L330).

RB75 with 6.247 engine

Where the 6.247 engine is fitted to the RB75 model a modified pipework layout is used in conjunction with a different pump. The pump is fully covered in Sub-section L322.

The larger diameter hose conveys fluid from the reservoir to the pump and is secured at both ends with hose clips. A pipe and hose assembly supplies fluid under pressure to the steering box. This pipe is secured at both ends by unions. The fluid is returned from the steering box to the reservoir via a 'T' piece which is incorporated in the return line from the pump to the reservoir. The return line is secured to the pump and reservoir by hose clips and by a union at the steering box.

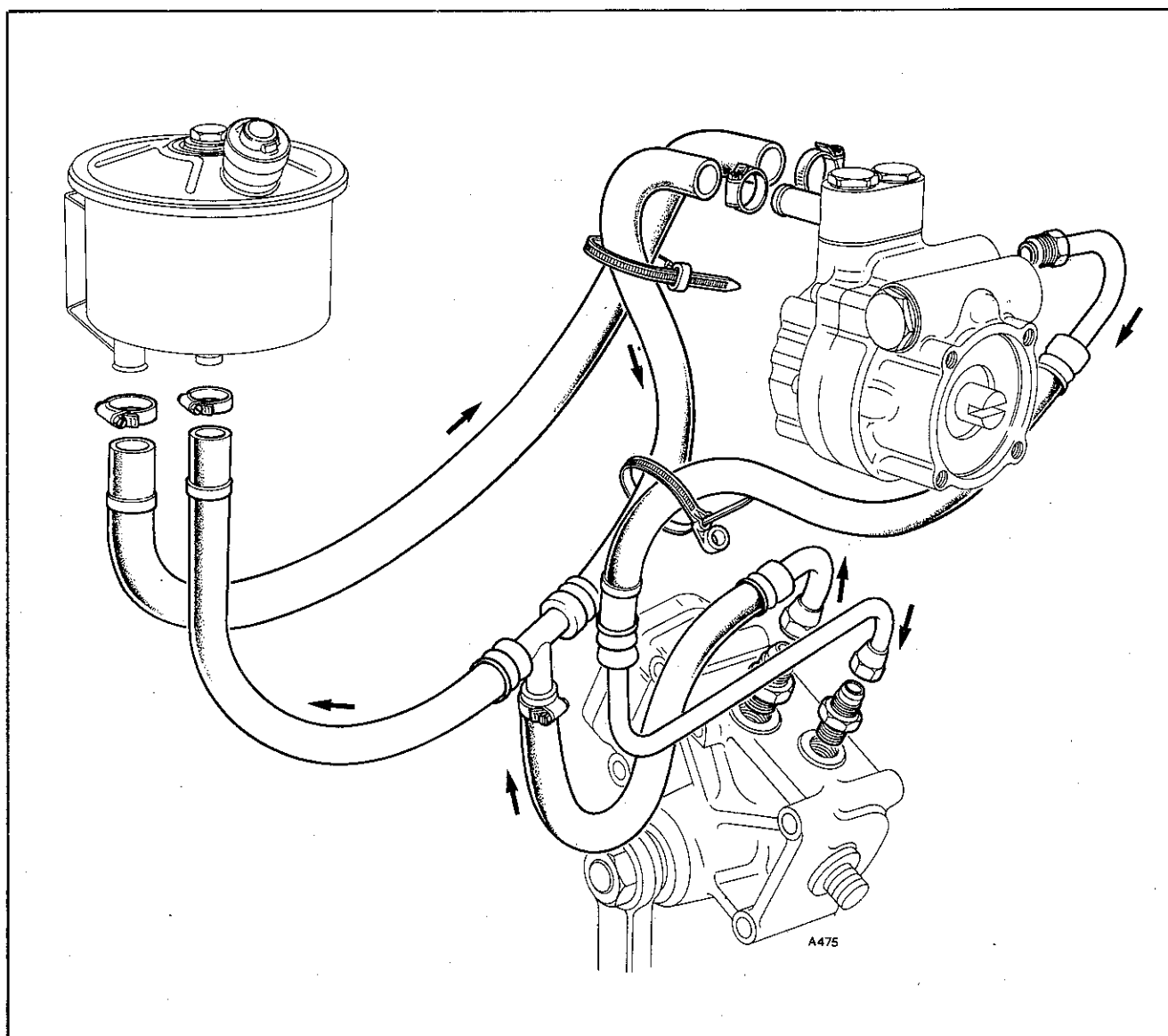


Fig. 2 Alternative Pipework used on RB75 with 6.247 Engine